



STOCKYARDS DESIGN OVERLAY DISTRICT

TABLE OF CONTENTS

UNDERSTANDING THE DISTRICT	1 - 6
Introduction	
History	
Objective	
Public Process	
Geography and Character of the District	
Boundary Map	
Maps of Corridors, Areas and Nodes	
ADMINISTRATION	7 - 9
Intent	
Relationship to Other Regulations	
Definition of Standards and Guidelines	
Certificate of Appropriateness	
Development Review Process	
URBAN DESIGN PRINCIPLES & DISTRICT CHARACTER	10 - 14
General Development Principles	
General Design Principles	
Corridors, Areas and Nodes	
ARCHITECTURAL STANDARDS & GUIDELINES	15 - 41
Building Edge	
Pedestrian Space	
Parking	
General Standards & Guidelines for all Districts Signage	
Building Edge	
Pedestrian Space	
Parking	
Height	
Signage	
Public Art	
Open Space	
Corridor/ Area Specific Standards & Guidelines	
Main Street Corridor	
East Exchange Corridor	
West Exchange Corridor	
Stockyards Boulevard Corridor	
Swift-Armour Area	
Marine Creek Corridor	
APPENDIX	42 - 47
A. Recommended Tree List	
B. Definitions	
C. List of Possible Local Regulations	
D. Designations for Resources within the Boundaries of the National Register	
- Listed Fort Worth Stockyards Historic District	
E. Boundary References	
F. External Building Materials List	

TASK FORCE MEMBERS

Eric Hahnfeld, Chair
Carlos Flores
Lori Gordon
Robert Gutierrez
Commissioner J.D. Johnson
Bob Adams*
Hub Baker
Gary Brinkley
Craig Cavileer
Marty Humphrey
Don Jury
Terry Kearney
Keith Kidwill
Billy Minick
Philip Murrin
Keith Powell

CONSULTANT TEAM

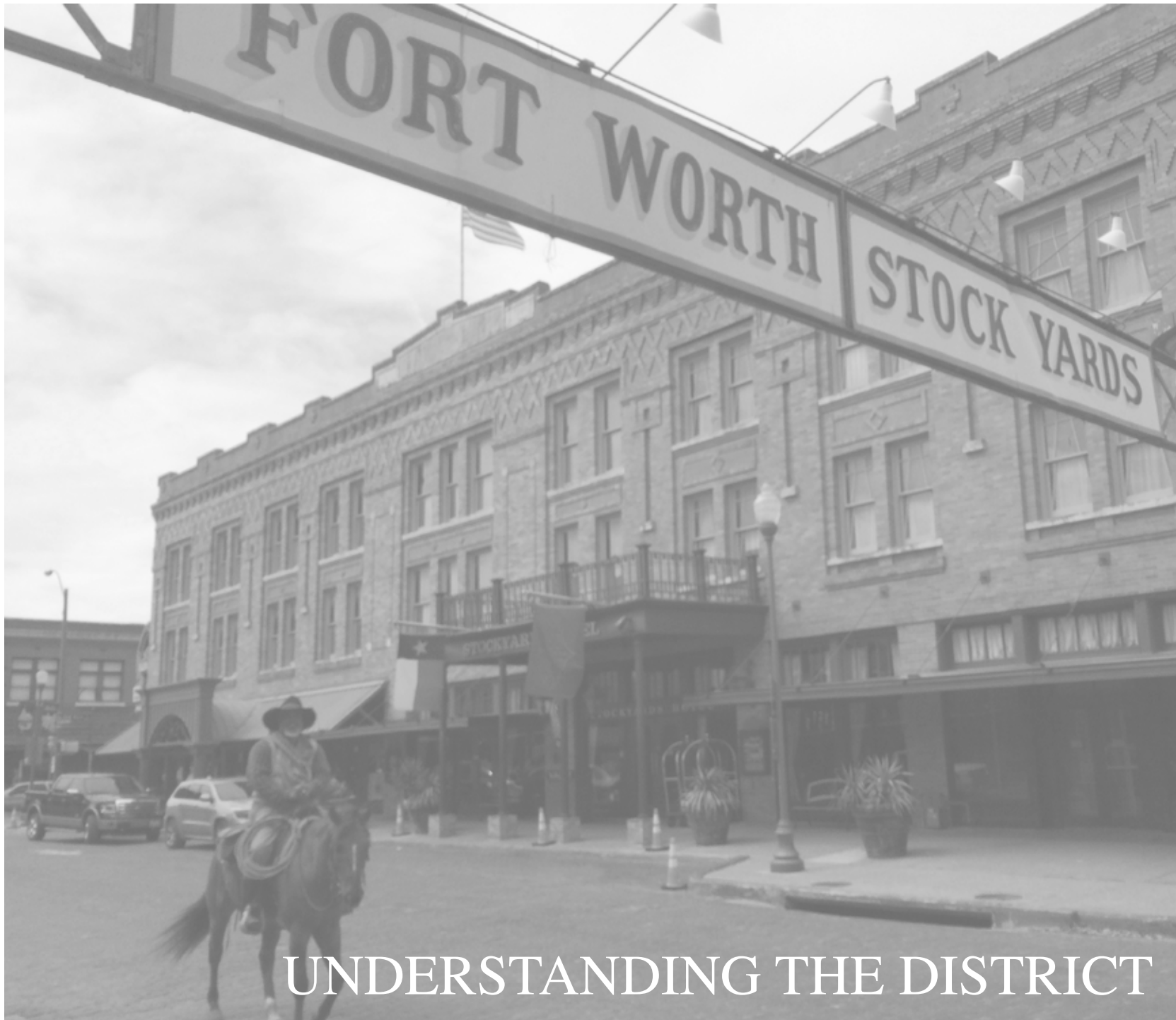
Randy Gideon
Cassie King
Martha Granger
Susan Kline

CITY OF FORT WORTH

Dana Burghdoff
Laura Voltmann

* Resigned from Task Force on 9/23/15





UNDERSTANDING THE DISTRICT

Understanding the District

History of the Stockyards

Fort Worth, Texas, became a prominent player in the national cattle industry after the Civil War, when the city became the final Texas stopover for resting and restocking supplies along the Chisholm Trail route, which was utilized to herd cattle from South and West Texas ranches into Kansas. The Trail continued north from Fort Worth, crossing the Red River and traveling through Indian Territory, before it met up with existing railroad infrastructure, stockyards, and meat-packing facilities in Kansas, which enabled distribution of highly demanded beef and other meat throughout the northern United States.

The arrival of the railroad in Fort Worth in the 1870's precipitated rapid expansion of the area now known as the Fort Worth Stockyards (the "Stockyards"). At this point, cattle and other livestock were purchased and shipped by rail to meat-packing facilities. By the early 1900's, however, two major meat processing and packing facilities, Armour & Co. and Swift & Co., were constructed, enabling the Stockyards to become a national hub for auctioning, slaughtering, packing and shipping of meat and livestock. During this time, large capacity animal pens were constructed, along with the historic Exchange Building and Coliseum, where the use of electric lights facilitated indoor cutting-horse contests and rodeos. The Stockyards grew into one of the largest livestock markets in the nation and continued its prominence in the economy for many decades.

After World War II, activity and resulting profitability began to decline in the Stockyards. Improvements in roadway infrastructure enabled the trucking industry to pull market share from the railroads, and a shift to county auctions in small towns kept livestock closer to ranches. The Armour plant closed in 1962. The Swift plant followed in 1971. Although the Stockyards deteriorated during the next decade, dining and entertainment venues such as Billy Bob's Texas rekindled interest in the area as a viable business center. In 1986 the renovation of the Stockyards Exchange Building and Cowtown Coliseum were instrumental in support of stockyards activity and rodeo. Over the last few decades, the Stockyards has continued to grow in profitability and prestige. Sheep and hog pens have been redeveloped into retail shops and restaurants, and a new hotel was recently built. Today, the commercial core of the Stockyards attracts international visitors and functions primarily as an entertainment, tourist, and shopping venue. Time, talents and resources of land owners and businesses have been invested in the development of the tourist destination it is today over the past 30 years. The "Cowtown" heritage is pervasive



throughout the area, and occupants continue to respect its historic nature. The authenticity of the culture and character of the Fort Worth Stockyards remains intact years after it ceased to operate as the region's central point for cattle and related product distribution. The Fort Worth Stockyards is one of the last standing stockyards in the United States and is listed in the National Register of Historic Places as a historic district. A list of contributing and non-contributing resources within the National Register District can be found in the Appendix. The National Register District boundary can be found on page 4. The definition of a Historic Resource can be found in the Appendix.

Objective

Development initiatives in the Stockyards will spur significant improvement of infrastructure, enable continued revitalization of existing original Stockyards facilities, and promote development of new structures in order to attract businesses, customers, tourists, and residents. The objective of the Standards and Guidelines for the Stockyards Design Overlay District (the "District") is to provide guidance and establish design standards for these activities.

Public Process

Public participation was a considerable part and foundation of the planning process. The public was presented with design and development principles to help guide the design process. The consultants gathered public input from numerous public meetings to start developing the guidelines. Planners consulted the public, major stakeholders in the area, and various interest groups. Through this public process, a council-appointed task force met with planning consultants and city staff to solicit

input and present recommendations. The Stockyards Design Overlay District Standards and Guidelines were subsequently created to ensure that the quality of new development supports the historical character and economic value of all property within the District.

Geography and Character of the District

The Fort Worth Stockyards is located just over two miles north of Downtown Fort Worth and represents one of three vertices of a triangle of activity in Fort Worth that is especially attractive to entertainment and tourism, the other two of which are Downtown Fort Worth and the Cultural District.

The Stockyards Design Overlay District roughly encompasses the boundaries for the city's existing mixed use zoning district classification of the Stockyards, the Public Improvement District for the Stockyards and the boundary for the Fort Worth Stockyards as designated by the National Register of Historic Places. The District is subdivided into six corridors/areas: Main Street Corridor, East Exchange Corridor, West Exchange Corridor, Stockyards Boulevard Corridor, Marine Creek Corridor, and the Swift-Armour Area. The district standards and guidelines are all inclusive and apply to the entire District. They include standards and guidelines for site layout and architectural character of gateways, buildings, pedestrian space, parking, streets, landscaping, lighting, signage and public art. Each corridor/area has specific requirements that apply, in addition to the District-wide standards and guidelines, ensuring that existing physical and aesthetic character of each corridor/area remains.





Exhibit A: District Boundary



ADMINISTRATION

Administration

Intent

The development standards and guidelines included in this document are intended to be clear, concise, and user-friendly. The development review process is intended to be predictable and flexible, and to facilitate the timely approval of conforming projects.

Relationship to Other Regulatory Documents

The Stockyards Design Overlay Development Standards and Guidelines “Standards and Guidelines” document is intended to minimize references to other sections of the ordinance. Developers are responsible, however, for reviewing and complying with all pertinent zoning and subdivision standards, including those not directly referenced in this document. All development must comply with all federal, state, county, or city regulations. Designated historic properties must comply with requirements associated with those designation, both local and state designations.

Definition of Standards and Guidelines

Development Standards are objective, measurable regulations, often illustrated through diagrams and sketches, with which all projects must comply. If a project of exceptional design is clearly consistent with the General Development Principles but does not conform to a certain development standard, the Urban Design Commission (UDC) may approve a Certificate of Appropriateness (COA) that cites the project’s compliance with those principles. Development Guidelines are more subjective statements through which the City proposes additional design strategies. The guidelines should be suitable for most projects, and developers should endeavor to ensure that guidelines are followed to the extent possible. City staff and the UDC will work with developers to explore design approaches that maximize conformance with guidelines.

Development Review Process

1. PROJECTS SUBJECT TO REVIEW – All private construction projects, with the exception of interior construction or exterior in-kind replacement work, are subject to review by the Planning and Development Department the Urban Design Commission (UDC) or both for compliance with the Standards and Guidelines. All buildings, streets, and public spaces by public entities are also subject to review.

2. ADMINISTRATIVE REVIEW VS. UDC REVIEW – Projects that are clearly consistent with all applicable development principles and that clearly conform to all standards may be approved administratively by the Planning and Development Department without UDC review. The UDC shall review projects that require interpretation or discretionary judgment with respect to the project’s compliance with standards and guidelines.

3. URBAN DESIGN COMMISSION REVIEW (UDC) – The UDC shall review projects that require interpretation or discretionary judgment with respect to the project’s compliance with Standards and Guidelines. The Zoning Ordinance specifies that UDC appointees should be knowledgeable about fundamental principles of urban design. Furthermore, at least six of the nine appointees shall be practicing professionals from the fields of architecture, landscape architecture, urban design or planning, real estate, or law. The Planning and Development Department serves as staff advisor to the UDC.

Certificates of Appropriateness

The Planning and Development Department or UDC will issue a Certificate of Appropriateness (COA) for approved public and private projects. The UDC may approve a COA if a project is consistent with applicable development principles but does not conform to a certain development standard(s) or guideline(s), provide however, the UDC may not deny a COA solely on the basis of nonconformance to guidelines. The COA for such projects must describe the projects’ compliance with applicable principles.

Required Information

The full list of required materials is included in the UDC application available from the Planning and Development Department. In general, the following information is required, as applicable:

- a. Site Plan
- b. Building Plans and Elevations
- c. Landscape Plan
- d. Material Specifications
- e. Plans and Specifications for Proposed Signs
- f. Description of Proposed Scope of Work
- g. Photographs of Site and Existing Conditions

Conceptual Phase Discussion

One of the UDC's roles is to provide informal guidance to developers of significant projects during the conceptual design phase. These discussions take place during monthly UDC public hearings and provide an opportunity for early UDC and staff input that should facilitate timely design review and approval of the final design phase. Such review by the UDC is non-binding upon their consideration of a final design. City staff encourages all public and private developers of significant projects to present their conceptual plans during a UDC public hearing.

Variances to Zoning and Subdivision Ordinances

The UDC is authorized to approve appropriate waivers to the Stockyards Standards and Guidelines, with the exception of waivers to height and setbacks, provided a project complies with applicable development principles. A request to vary from height and setback standards must go to the UDC for consideration and a recommendation prior to review for a determination of the variance request by the Board of Adjustment. Vacations of public rights-of-way require approval by the City Plan Commission and the City Council. Before a request to vacate right-of-way will be placed on the Plan Commission's agenda the request must be reviewed and a recommendation given from the UDC. The UDC is authorized to approve exceptions to roadside design standards.

Historic Properties

The preservation and adaptive reuse of historic buildings should be a focal point of the redevelopment of the Stockyards. The goal of historic preservation is to preserve and adaptively reuse the remaining historic and architectural resources and to ensure that new construction complements the historic and architectural character of the Stockyards.

Specific local historic designations within the Stockyards include:

1. Demolition Delay (DD) – protects properties from demolition for up to 180 days while a search for an alternative use for the building is conducted.
2. Historic and Cultural Landmarks (HC) – protects the structure indefinitely and provides the owner property tax incentives for rehabilitation of the building.
3. Highly Significant Endangered (HSE) – provides the highest level of protection and accordingly the highest level of tax incentives.

Projects Requiring Review by the Texas Historical Commission:

1. State Antiquities Landmarks
2. Recorded Texas Historic Landmarks
3. Any Federal undertaking with the potential to affect properties listed in, or eligible for listing in, the National Register of Historic Places, in accordance with Section 106 of the National Historic Preservation Act.
4. Any project by the City of Fort Worth, or other political subdivision of the State of Texas, directly affecting a designated historic site or district, or recorded archaeological site, in accordance with the Antiquities Code of Texas.
5. Any project seeking state and/or federal historic rehabilitation tax credits.

Note: City staff is required to notify the Texas Historical Commission of any work taking place on a property listed in the National Register of Historic Places.



URBAN DESIGN PRINCIPLES &
DISTRICT CHARACTER DESIGN
PRINCIPLES

General Development Principles

Vision

Members of the task force expressed that they envision a district in North Fort Worth that reflects the city's western culture and character. Development in the area should have a high level of quality and aesthetic character that will support the continued economic sustainability of the area while ~~retaining~~ complementing the historic features of the streetscape and built environment. The Standards and Guidelines will provide the direction for this development while maintaining a cohesive sense of western culture. The vision shall be attained by focusing on primary design elements such as site planning, architectural character, streetscape, lighting, signage and landscaping.

General Development Principles

The following principles are the building blocks upon which the Standards and Guidelines are created and through which the development can best represent the desires of the community.

Maintain a Sense of Place: The Stockyards Design Overlay District shall maintain its distinct sense of authentic western culture and extend its "brand" throughout. It should be recognized that the area is made up of six separate development corridors/areas, which are reinforced through the use of unique, area-specific site layout and architectural standards. By promoting a cohesive sense of development through architectural design and pedestrian-friendly walkability, these Standards and Guidelines can enhance the existing sense of western pride and identity.

Maintain a High Standard of Development: The Standards and Guidelines will promote high quality and exceptional design, encourage creativity, and support the diversity of a mixed use environment. It will ensure positive growth and development of quality site layout, architectural character, streetscape, lighting, signage, and landscaping. The development review process will promote flexibility within the Stockyards Design Overlay District.

Protect Investment: The Design Standards and Guidelines for the Stockyards Design Overlay District shall protect the investment and support the success of current and future landowners, investors, and users by providing opportunity for growth and attracting new businesses and consumers. It will provide enhancement of the character of existing business and commercial areas as well as to the visual and aesthetic character of the corridor.

Preserve and Restore Historic Features: One of the key objectives of the Standards and Guidelines is to preserve the ~~history~~ historical fabric and character of the Stockyards. The Stockyards is listed in the National Register of Historic Places as a historic district. Within this district are buildings, structures, and landscape features that contribute to the District's sense of place and historic character. These buildings and other significant structures and resources must retain a respectful presence in the District, as they cooperatively act as the anchor which ties the development together. Adaptive reuse and preservation must be encouraged. (See the City of Fort Worth's preservation program.)



Urban Design Principles and District Character

Design Principles

Urban design principles provide the foundation for shaping the relationship between the built environment, nature, and people. The following principles will act as the reference point for cohesive development and will guide the UDC and the staff's review of a project's compliance with the Standards and Guidelines.

1. **Authenticity**

The Fort Worth Stockyards is one of the last remaining stockyards in the United States. As such, it not only represents a unique heritage for the city, but also plays a significant role nationally and internationally in preserving history. The western character of Fort Worth should be the general concept of all designs. The look and feel of the development should have a unified consistency. Careful studies of existing historic structures should be conducted for restoration or compatibility purposes, and historic use of the land should be noted and incorporated into the design if possible.

2. **Strengthening the Brand**

The design of the District should reflect the highly recognizable features of the existing commercial core of the Stockyards, both tangible and intangible. Physical aspects such as building materials and shapes, use of green spaces, and visual elements such as signage should be referenced through the District in some capacity in order to fully develop the feel of the Stockyards community. Intangibles such as the feel of the Stockyards history can be conveyed by means of public art.

3. **Preservation and Re-use**

Preservation of the built environment within a defined geographic location such as the Stockyards not only provides a link to the past but can also create economic benefits by increasing the property value of the area as a whole. Historic infrastructure should be preserved if possible, renovated, or adaptively reused.

4. **Quality**

Quality is defined as a distinctive attribute that has a degree of excellence when compared to the norm. From material usage to construction to long-term maintenance, levels of high quality should be the aim when designing within the District.

5. **Flexibility**

While efforts should be made to achieve the general principle of authenticity and to create a consistent feel within the District, flexibility should play a significant role in the design process. Creative and resourceful solutions with compatible local character are encouraged.

6. **Compactness and Walkability**

Movement within and between the character zones of the District should be easy and seamless. Walkways should be inviting and provide sufficient shade. Public spaces should be interesting and engaging, and wayfinding should make it easy to navigate.



7. **Sustainability**

Designs within the District should be planned with long-term viability and success in mind. New construction should make use of readily accessible durable materials and innovative construction techniques to support the sustainability of the District. Existing historic structures should be restored or rehabilitated in accordance with the Secretary of the Interior's Standards for the Treatment of Historic Properties and other guidelines provided by the National Park Service.

8. **Safety**

Safety is a significant component of any development. The security of both people and livestock should be an integral part of the design of the District. A focus on elements such as lighting and traffic circulation can enhance this safety.

9. **Accessibility**

The District exists predominately as a tourist attraction and draws a diversity of individuals. Designs within the District should take into account and attempt to alleviate challenges that may act as a barrier to those with disabilities and comply with state and federal accessibility regulations.



Corridors and Areas

The Stockyards Design Overlay District Standards and Guidelines apply to the area depicted in the boundary map in section one (Exhibit A on page 4). While a goal of the Standards and Guidelines is to establish a strong cohesive identity for the Stockyards, it is recognized that the district is made up of distinct development corridors and areas. These corridors and areas differ greatly in density and in use of the existing development, architectural style, and in the amount of space available for development or redevelopment. The expression of these corridors and areas will establish a framework for the western experience within the entire District. Subdivision of the District into these areas will allow the type of development to vary in a coordinated manner such that the overall historical quality and western character of the District is maintained.

Main Street Corridor

The existing commercial center of the Stockyards is located less than three miles west of I-35W. The closest major exit off this interstate highway is NE 28th Street, which is roughly the northern boundary for the District. As seen on the boundary map, this street intersects Main Street near the northwest corner of the District (the Main Street Gateway North Node). The Main Street Corridor consists of the main access roadway into the western portion of the District. It encompasses the transitional commercial corridor which is the length of Main Street between NE 28th Street to NE 22nd Street. Also included is a retail strip oriented to North Main that is approximately 1000 feet in length stretching east from the Main Street Gateway North Node on NE 28th Street to the start of the bridge, which spans the railroad. This corridor will provide direction into the Stockyards, while simultaneously building excitement and momentum for visitors and occupants. Buildings along this corridor should support the character of the Stockyards and the historic buildings along Main Street with compatible design themes while conforming to existing zoning standards.

Main Street Gateway North Node

A subset of the Main Street Corridor is the Main Street Gateway North Node. It lies at the intersection of Main Street and NE 28th Street, which is near the northwestern corner of the District. It marks the primary juncture into the Stockyards for many visitors, as traffic from I-35W will feed into this node. This node provides the opportunity for an enhanced gateway experience and sense of arrival to the Stockyards.

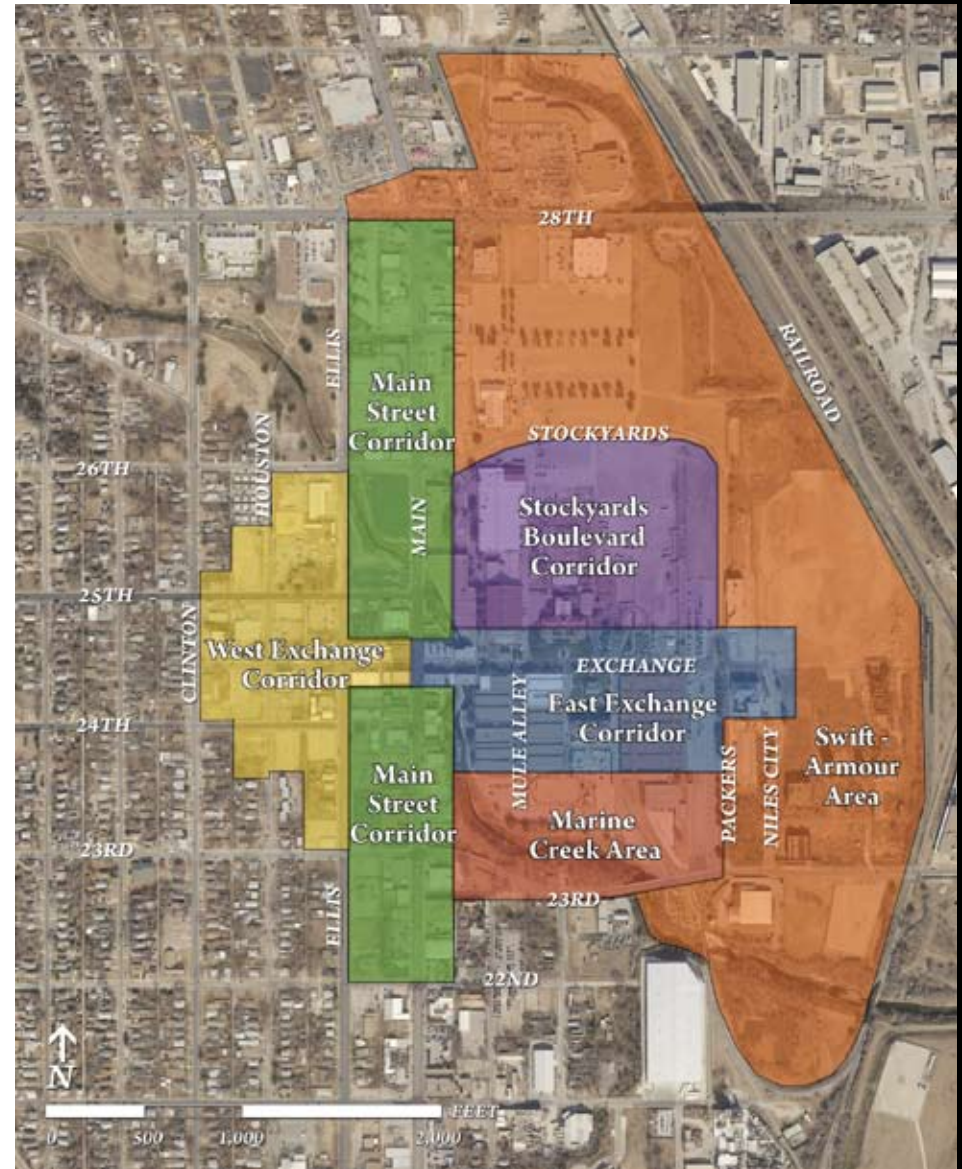


Exhibit B - Corridor/ Area Map

Main Street and Exchange Node

Another subset is the Main Street and Exchange Node, which is currently and will remain the most historic heart of the Stockyards. It is located at the intersection of Main Street and Exchange and is the focal point which sets the tone for the experience in the District. The historic character of this node is strongly intact. The sensitivity of development in this area will have significant impact on development patterns in the Stockyards.

Main Street Gateway South Node

The final subset of the Main Street Corridor is the Main Street Gateway South Node, which is located at the intersection of Main Street and NE 23rd Street. This node will capture traffic flowing from additional points of interest and activities in Fort Worth, namely Downtown, the Cultural District, and the Panther Island development. Arrival at this gateway will announce entry into the Stockyards from the south. Development along Main Street will set the tone for the physical Stockyards experience.

West Exchange Corridor

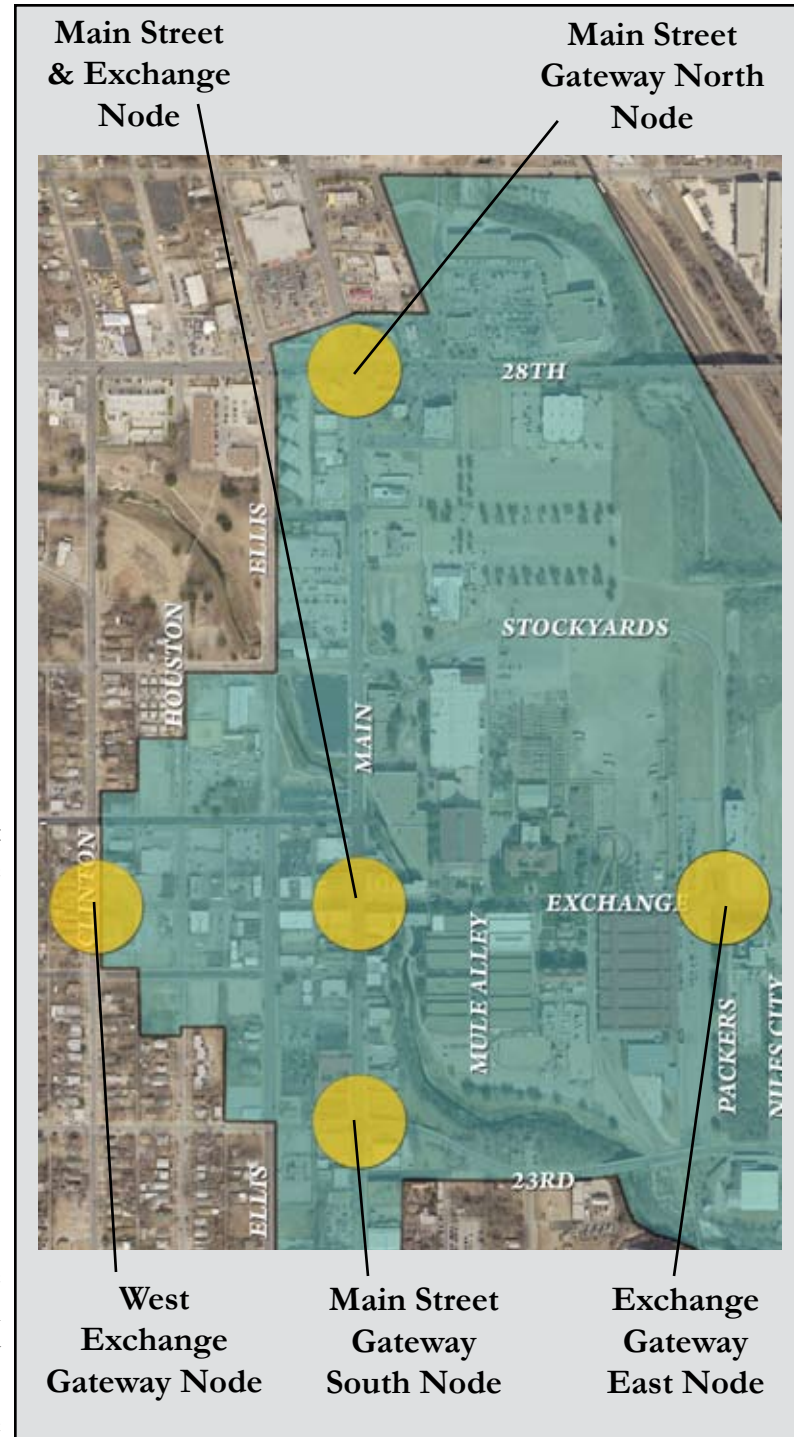
This corridor encompasses the area to the west of Main Street along and surrounding West Exchange Avenue, as shown in Exhibit B. The majority of existing development occurs along West Exchange within the first three blocks from Main Street and is comprised of restaurants, stores, and entertainment venues. This corridor will be responsible for providing a transition to the neighboring properties to the west.

West Exchange Gateway Node

A subset of the West Exchange Corridor is the Exchange Gateway West Node, which is located at the intersection of West Exchange Avenue and Clinton Avenue. This node defines the western most edge of the District and will serve as the gateway to the residential area to the west.

East Exchange Corridor

This corridor is the core of the District and contains some of the most treasured original Stockyards structures such as the Exchange Building, Coliseum, mule barns, cattle pens, and adaptably re-developed Stockyards Station. Also included are more recently built structures such as the Visitor Center and a nationally recognized full-service hotel. As indicated in Exhibit B, the corridor stretches from Main Street to the east along East Exchange Avenue and terminates at the



base of the stairs of the Swift-Armour Plaza. The most historic recognizable buildings/structures front on Exchange Avenue, reinforcing the significance of development along the corridor. Existing structures should be renovated or adaptively reused if possible, keeping the district character while optimizing safety and utilization. Any new development in this corridor will need to complement the existing architectural character by carefully addressing setbacks, scale and material. So as not to overwhelm original structures, focus should be placed on creating compatible, but not competing, structures. A strong focus on pedestrian walkways and spaces will be essential in this area.

Exchange Gateway East Node

A subset of the East Exchange Corridor is the Exchange Gateway East Node. It lies roughly at the intersection of East Exchange Avenue and Stockyards Boulevard. As traffic circulation requires that traffic flow from west to east along Exchange Avenue, this node marks the transition from the East Exchange Corridor into either the Stockyards Boulevard Corridor to the north or the Swift-Armour Area to the east.

Stockyards Boulevard Corridor

This corridor has been traditionally anchored by a nationally recognized entertainment venue, but mainly consists of surface parking, as the cattle pens which once covered the area have long been removed. Exhibit B shows the boundary for the Stockyards Boulevard Corridor. The lack of existing infrastructure in this corridor provides a uniquely flexible opportunity for new development, including pedestrian spaces and parking. A new entrance into the Stockyards could be created off the highly travelled NE 28th Street into this area, which could lead quickly to parking facilities. Retail, entertainment, hotel, office space and residential may be appropriate uses for this corridor.

Marine Creek Area

Marine Creek flows along the southwestern edge of the District, as displayed in Exhibit B. Similar to the Stockyards Boulevard Corridor, minimal development exists within the Marine Creek Area. The opportunities to take advantage of the natural beauty of the creek will be boosted by the proposed nearby Panther Island development, as a channel lock will be constructed. This will enable water travel along a navigable waterway which will connect the Stockyards to Panther Island and downtown Fort Worth. Waterfront oriented development will encourage a pedestrian-friendly environment with uses such as dining, retail, office space and residential. A continuation of development to the north could be anchored on this south end of the District by offering similar development opportunities.

Swift-Armour Area

The Swift-Armour Area is located on the easternmost side of the District, as depicted in Exhibit B. It is comprised of the former Swift & Company and Armour & Company meat packing plant sites. Over the years, much of the packing plant structures were lost to fires and demolition. The effects of nature, vandals, and vagrants have added to the erosion of the area and limit the possibility of restoration. Currently, the only functioning structure is the Swift & Company headquarters building, which was first repurposed into a restaurant and then extensively restored into office space. Much of the area is elevated from adjacent Stockyards corridor by a small hill. This topographical difference provides a natural transition into the Swift-Armour Area, which could be used for new development with a variety of scales and uses.



Architectural Standards and Guidelines

Guidance

The base standards for the entirety of the District will be those of the existing city zoning ordinance(s). Whenever there appears to be a conflict between the Standards and Guidelines and other sections of the City of Fort Worth Zoning Ordinance or other applicable regulations, the more restrictive requirement shall prevail, unless otherwise provided in this document.

Standards and Guidelines for All Corridors and Areas

The following standards and guidelines apply to all corridors and areas. Standards and guidelines specific to individual corridors and areas will follow.

Building Standards and Guidelines

General Setbacks Guideline

- Buildings should have a setback of no more than 5 feet from the back of the sidewalk. If a larger setback is desired then the case can be reviewed by city staff.

Building Orientation Standards

- Building primary face or entry shall orient on the street or other public spaces.

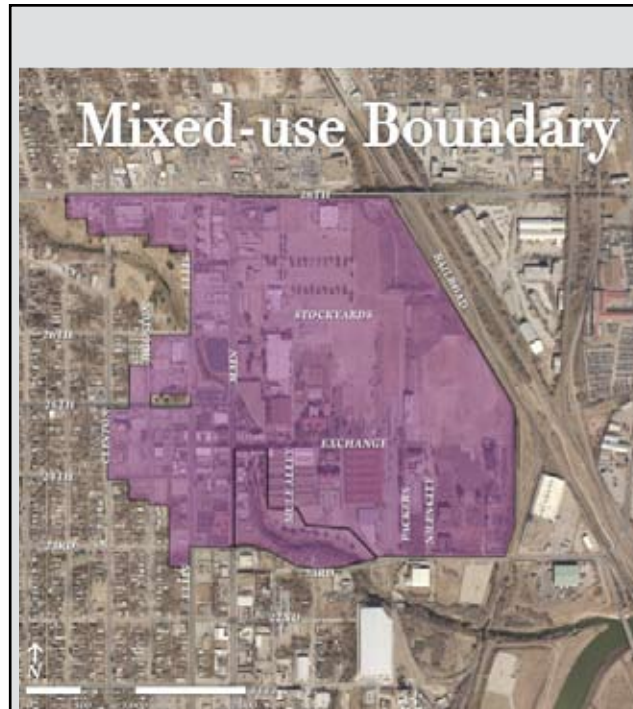
Arcades and Overhangs Standards

- The addition/replacement of awnings on historic buildings shall be based on historical evidence/documentation. If no documentation exists, awnings shall either be retractable canvas awnings or flat metal awnings located between the storefront windows and the clerestories. Typically, they are attached by cables above the awning. Cables shall be attached at mortar joints to avoid damage to brick.
- Wood shed/shingle awnings with wood or other types of columns are discouraged.

Awnings and Overhang Guidelines

Awnings for primary entrances are encouraged with the following exceptions:

- On buildings that incorporate an arcade into the architecture.
- On buildings that provide a recessed entrance.
- Where the awning or cover may interfere with vehicular and loading entrances for the building.
- When the health and safety of the public is jeopardized.
- Where it may interfere with architectural style and integrity of the structure or obscure architectural details on the façade.
- Self-illuminated sign awnings are discouraged.



Base MU-2 Zoning Boundary



Existing Overhang and Building Setback on Exchange Avenue

- Awnings or canopies should not be constructed on historic buildings where there is no evidence to support the existence of such a feature during the building's period of significance.

Pedestrian Passage Standards

- No block shall have more than 400 feet of street frontage without a pedestrian passage. UDC review shall be required, if staff determines that the block is being developed in a piecemeal fashion over time by multiple property owners, and that the provision of a pedestrian passage is impractical because it places an undue burden on a single property owner.
- Width – minimum of 20 feet. A smaller passage may be approved on two conditions:
 - If the developer has more than one passage per block and,
 - If the buildings framing the passage is not higher than 3 floors.
- Shall be accessible through a public pedestrian access easement.
- Shall extend through the full block to provide connections to adjacent streets.

Façade Standards

- Changes to existing building facades shall complement, but not mimic existing architecture, architectural features and building footprint. The ground level shall be designed in a fashion to allow conversion to retail or other active uses if they are not feasible at the time of opening.
- Ground floor glazing and/or fenestration shall be provided in new buildings.
- Overly tinted, reflective, or opaque glass is not allowed on the ground floor of buildings.
- The facade of buildings shall include architectural relief at least every 30 feet. This relief may include, but is not limited to the following:
 - Doors and Windows
 - Change in depth
 - Columns or posts
 - Changes in materials
- All new buildings shall differentiate between the first and second floor.

Façade Guidelines

- In new buildings continuous facades greater than 30 feet in length should be broken up with articulation or other architectural treatments.
- For buildings that terminate an axis, vertical architectural elements are encouraged because they provide an excellent way to terminate the axis.
- Operable windows are strongly encouraged for all buildings.
- Painting of existing unpainted brick is highly discouraged.



Example of a pedestrian passage



Existing facade on Exchange Avenue

Entry Standards

- Windows, floors, stoops, and porches shall front the street.
- Primary building entrances shall be clearly articulated using any of the following:
 - Awnings
 - Canopies
 - Recessed entry
 - Other similar treatments may be considered on a case by case basis by city staff or the UDC.
- Primary pedestrian building entrances shall be located on the street frontage of the building. For buildings fronting other public spaces, the primary pedestrian entrance shall be oriented to and accessible from the public space.
- Individual retail entrance – each retail use with exterior ground level exposure along a street or public space shall have an individual public entry from the street or public space.

Entry Guidelines

- Where office and retail/entertainment spaces coexist consider, placing the retail entrances and prime exposure at the corners and the office entrances at mid-block locations.
- Corner entrances are encouraged for strong visibility at intersections.
- Entrances to corner buildings with ground floor retail should be located at the corner.

Fencing Standards

- Private patio, yard, or sidewalk dining area shall be bordered by open railing measuring maximum 4 feet from ground level.
- Railings for outdoor dining shall not extend into the Pedestrian Way.
- Chain-link fences are not allowed.
- Front yard perimeter fencing prohibited – conventional gated complexes with perimeter security fencing along public streets are prohibited. Specifically, exterior security fences and gates that are located along public streets, along private streets or walkways that are publicly accessible through a public use easement, or along publicly accessible open space shall not extend beyond building façades; i.e., these fences shall not be located in the area between building façades and the property line.
- Privacy fences – a front fence or railing not exceeding 4 feet in height may extend beyond building façades if the fence encloses a private patio, yard, or sidewalk eating area. The fence is not permitted to encroach on a required Pedestrian Way, as defined on page 20.



Existing Facade on Main Street



Existing Corner Entrance on Main and Exchange

Roofing Standards

- Roof shape and roofing materials shall be harmonious with existing buildings and overall building design.
- Parapets shall be high enough to screen all roof mounted equipment from the view from grade level.
- Wind and solar energy devices visible from the public right-of-way require city staff approval.

Equipment and Service Areas Standards

Building equipment and service areas include but are not limited to the following:

- Rooftop equipment
- Mechanical and electrical equipment and conduit
- Ducts
- Piping
- Fire equipment
- Water backflow devices
- Trash facilities
- Recycling facilities
- Utilities
- Satellite dishes
- Solar collectors
- Antennas
- Loading and unloading areas
- Drainage facilities
- Building equipment and service areas shall be designed and located so that they are not the primary building feature and do not interfere with pedestrian and/or vehicular circulation.
- Equipment and service areas shall be combined when possible.
- Equipment and service areas shall be incorporated into the design of the building.
- Dumpsters shall be located at the rear of the property out of site from any street and screened from public view.
- Rooftop equipment shall not be visible from neighboring properties at the same level or below.
- Visible equipment shall be constructed of non-reflective material and screened to the greatest extent possible.
- Screening materials shall be chosen to blend with and complement the architectural style of the building.

Temporary Construction Facilities Guidelines

Temporary construction offices, storage sheds, trailers, barricades, fences, and the like will be allowed as necessary during the construction of a permanent building.

Guidelines:

- Such facilities shall be placed as inconspicuously as possible to the general public and/or neighboring property owners.
- Temporary construction facilities are approved by staff.

Drive-Through Standards and Guidelines

Windows and stacking lanes

- Standard: Drive-through windows and stacking lanes shall not be located within the front yard setback or along facades that face a street.
- Guideline: Where possible, they shall be located to the rear of buildings.

Circulation

- Standard: The design and location of the facility shall not impede vehicular traffic flow and shall not impede pedestrian movement and safety.
- Guideline: Shared driveways and/or driveways located off of non-arterial streets should be used.

Screening

- Standard: architectural elements, landscaping, and/or other screening elements shall be used to minimize the visual impacts of the drive-through facility.

Pedestrian Space Standards and Guidelines

Intent

Expand comfortable sidewalks and energized streets in the District.

Objectives

- Focus pedestrian circulation at the street level.
- Use sidewalks and streets as a linear park system.
- Reinforce the Stockyards as a market place.

Elements of a Streetscape

Frontage Zone

The Frontage Zone is located immediately adjacent to buildings, walls, fences, or property lines fronting a street or public space. This zone typically includes the following:

- Landscaping/ planters
- Awnings and news racks
- Benches
- Outdoor café seating
- Building signage

Pedestrian Way

The Pedestrian Way serves as the area dedicated to walking and shall be kept clear of all fixtures and obstructions. The clearance provided in the Pedestrian Way should generally be straight for the convenience of all pedestrians, but especially for the impaired, between the Frontage Zone and the Furnishing Zone.

The standard widths for the Pedestrian Way shall be a minimum of 8 feet. Where sufficient right-of-way and/or public access easement exists, the Pedestrian Way shall be greater in width.

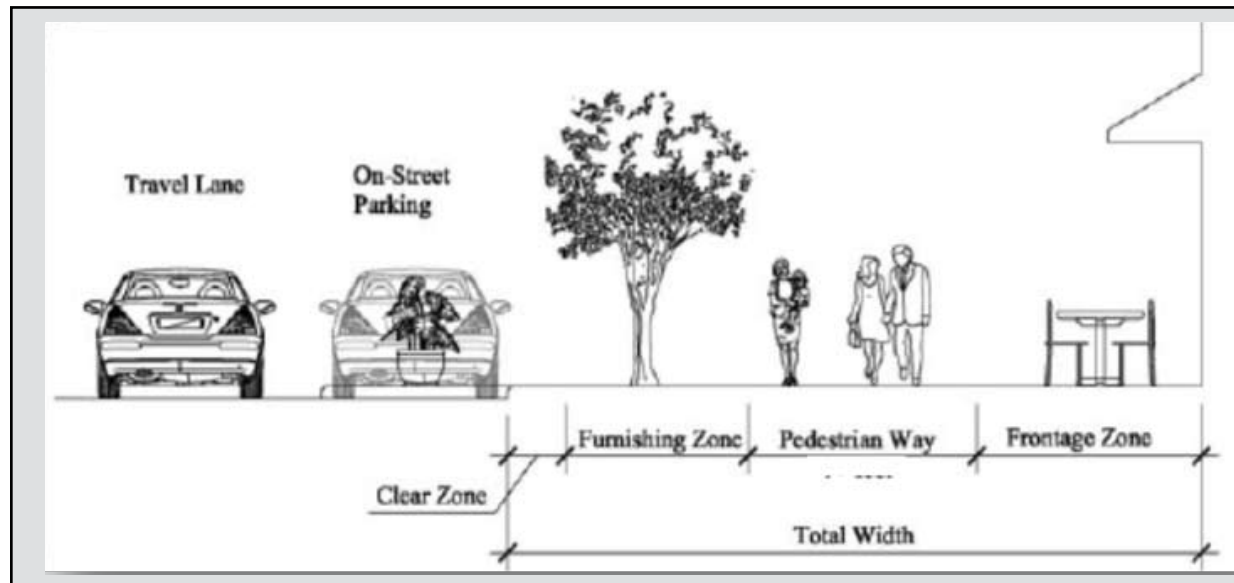
Furnishing Zone

The Furnishing Zone lies between the Pedestrian Way and the Clear Zone. This zone includes the following:

- Street trees and utility poles
- Parking and transit signage
- Bike and news racks
- Benches
- Transit shelters
- Waste receptacles
- Outdoor seating depending on sidewalk width
- Wayfinding signage

Clear Zone

The Clear Zone separates the Furnishing Zone from the street. The Clear Zone includes the width of the curb and may contain extra space for the unloading of passengers or freight. The minimum width of the Clear Zone is 2 feet. The width should increase to at least 3 feet where pedestrians or freight loading is expected and may conflict with obstacles in the Furnishing Zone.



Landscaping

Intent - Increase the streetscape experience and quality of life for business owners, residents, and visitors in the Stockyards.

- Objectives: Provide shade and a sense of place through the addition of more trees and/or landscaping.
- Increase comfort for pedestrians.
- Visually enhance stark walls through the use of plant material.

Landscaping Standards

- Street trees shall be located in the Furnishing Zone next to the street (within 2 to 3 feet of the curb)
 1. Exception: On sidewalks greater than 18 feet, a double row of trees is allowed.
- Irrigation shall be provided to ensure the life of the tree.
- Tree limbs shall be pruned to a clearance height of 8 feet.
- Trees shall be selected to fit the size and scale of the setting.
- Trees shall be a minimum of 3-inch caliper.
- Planting distance between trees shall be a maximum of 40 feet.
 1. Exceptions:
 - Curb cuts.
 - Fire hydrants.
 - Fixed infrastructure elements.
 - Primary building entrances.
- Where necessary, spacing exceptions may be made to accommodate mature trees, curb cuts, fire hydrants and other infrastructure elements.
- Trees shall be planted flush with the sidewalk surface using tree grates.
- Tree grates shall be adjustable to allow for tree growth and shall be securely attached.
- Tree grate openings shall comply with Texas Accessibility Standards. In areas with narrow sidewalks, suspended paver grates that allow for the placement of pavers directly over the tree planting area.
 1. Exceptions
 - If infrastructure prevents the use of at grade planting, a raised planter may be used or,
 - If crushed stone is justified as being the best design solution.
 - Other techniques will be considered on a case by case basis by the UDC.



Example Streetscape with clear and furnishing zones



Example of an adjustable tree grate

Landscaping continued:

- Developments that are set back from the build-to line shall incorporate landscaping such as shrubs, grasses, perennials, or ground covers.
- Durability of materials at street level is important to the Stockyards. Therefore, pots and planters shall be a minimum of 50% clay, concrete, ceramic, resin, or masonry materials.
- Pots shall be located in the Furnishing or Frontage Zones and shall not interfere with Pedestrian Way.
- Potted plants shall be compatible in scale and design with the immediate surrounding area.
- Developments must conform to the tree preservation standards as set forth below. Canopy of street trees can be applied towards mitigation requirements.
 1. Preservation of Significant Trees – Significant trees at least 30 inches in diameter (94.25 inches in circumference) can only be removed by permit. Criteria for the permit will be one of the following conditions.
 - Retention of other existing trees on the same site with a canopy area 1-1/2 times the area of the specific tree's canopy, or
 - Removal will be granted if planting of new trees at five (5) times greater in canopy area than the removed specific tree canopy, or
 - Payment into the tree fund based upon the total diameter of the specific tree times \$200 per diameter inch, or
 - UDC approved plan that mitigates the removal of the large tree. Replacement of any tree preserved and that dies within five years due to construction or development activities will be the responsibility of the original applicant. Replacement will be new trees with a minimum of 3 inches each in diameter and equal to five times the lost canopy. Tree replacement will be guaranteed for a period of two years.
 2. Trees in Surface Parking Lots – All new construction projects shall provide at least 40% tree canopy coverage of surface parking lots.
 3. Tree Protections and Warranty/Replacement – The provisions of Zoning Ordinance Chapter 6, Article 3, Section 6.301.K.6 apply. These provisions ensure that sufficient care is taken during construction to protect existing trees, and that any preserved or newly planted tree required by the provisions of Sections 5.G.1. and 5.B.3. in this document shall be replaced if it dies within 5 years.
- Public and private development shall provide shade trees within the Furnishing Zone.
- Trees shall be planted at the time of development, although the planting schedule may be extended to allow planting during the fall/winter season. (Between November and February)

Examples of approved Street Trees:



Maidenhair, Ginkgo Biloba



Chinese Pistache,
Pistachia chinensis

Surface Parking Landscaping Standards

- Surface parking lots shall be delineated from the sidewalk with landscaping, low walls or fences, or decorative post and chain. Chain-link fences are not allowed.
- Parking lot screening walls or shrubs shall not exceed 4 feet in height.
- Large street trees shall be planted adjacent to parking lots and walkways to help screen and shade.
- All parking lots shall have minimum canopy coverage of 40%.
- A list of approved canopy trees can be found in the Appendix.



Lighting

Pedestrian Lighting Standards

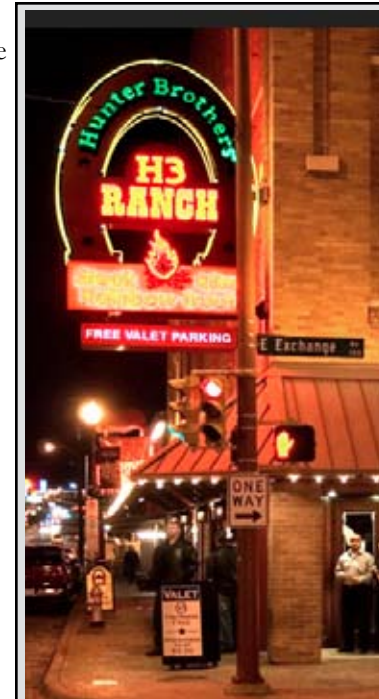
- All new development and redevelopment shall provide pedestrian lighting.
- Lights shall be located in the Furnishing Zone as defined in Elements of a Streetscape. on page 21. Spacing requirements can be adjusted to accommodate the following:
 1. Exceptions:
 - Mature trees
 - Curb cuts
 - Fire hydrants
 - Fixed infrastructure elements
 - Steps and existing building
- Pedestrian lights shall be located approximately at the midpoint between two trees and spaced a maximum of 50 feet apart. [If no pedestrian lights or street trees exist on the project's block face, measure from the end of the block to determine spacing. (For trees: begin 40 feet from curb intersection to accommodate public open space easement requirement.) If trees or lights exist, measure from existing trees or lights.] Where necessary, spacing exceptions may be made to accommodate mature trees, curb cuts, fire hydrants and other infrastructure elements.

Pedestrian Lighting Guidelines

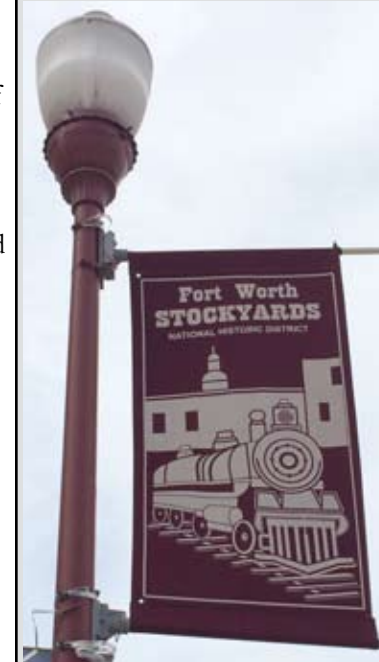
- Continuity of lighting style throughout the district is encouraged.
- Lights should be durable and easy to maintain or replace.

Exterior Lighting Standards

- Lighting shall be designed in a manner to avoid disturbances and glare onto adjacent properties.
 1. Exception
 - If an applicant can demonstrate that lighting will not contribute to light pollution and glare they may be granted approval by the UDC.



Main & Exchange at night



Existing Acorn Light

Furnishings Standards

- Permanent elements shall not be located in the Pedestrian Way (see Page 20).
- Benches shall be compatible with other benches along a block face.
- Benches shall be constructed of durable material.
- Other streetscape furnishings such as bike racks, trash cans, and other movable objects shall be compatible with existing furnishing along the block.
- Exhaust/venting, trash containers, and noise-generating mechanical systems shall not be placed adjacent to the walkway in a manner that diminishes the comfort of pedestrians.
 1. Exception for existing structures - The applicant can clearly demonstrate to the UDC that it is physically impossible or not economically feasible based on the original design of the building.
- Ventilation devices shall not be located in the Pedestrian Way
 1. Exception - The applicant can clearly demonstrate that it is a prior condition based on existing infrastructure.

Furnishing Guidelines

- Bike racks, trash bins, and seating should be incorporated into streetscape designs on streets planned for high levels of pedestrian activity. Continuity of style throughout a corridor or area is encouraged. These elements should be durable, cost effective, and easy to maintain.

Sidewalk Standards

- The width of the sidewalk shall be consistent with the prevailing street pattern in the immediate sidewalk provided that no new sidewalk shall be less than 10 feet wide.
- All sidewalks shall have a minimum of 8 feet unobstructed Pedestrian Way.
- Sidewalks shall be installed along all street-facing façades.
- On these sidewalks, bricks shall be incorporated as a prominent pattern.
- Concrete or a combination of brick and concrete shall be used for all sidewalks in the District, which are adjacent to non street facing façades.
- Synthetic surface treatments are not allowed.
- Asphalt is not allowed.
- The Pedestrian Way shall have walkways a minimum width of 5 8 feet and shall comply with ADA and Texas Accessibility Standards. Exemption: Minor renovation projects on properties with existing sidewalks.

Sidewalk Guideline

- Bringing restaurant/retail activities out to the walkway is strongly encouraged, where unobstructed pedestrian walking is maintained.



Example of possible furnishings



Existing brick on Exchange Avenue

Parking Standards and Guidelines

On-site Surface Parking Guidelines

- On-street parking is encouraged.
- Environmentally sound methods should be considered to reduce storm water runoff.

Screening Guidelines

- Ornamental wood screening walls may be used but wood requires consistent maintenance to retain its aesthetic appeal. Low brick walls or low-growing shrubs can provide parking lot screening.

Parking Structure Standards

- The amount of street frontage devoted to a parking structure shall be minimized by placing its shortest dimension(s) along the street edge.
- Parking garages shall be designed to provide retail and/or office uses at the street level. If these uses are not feasible at the time of development, design of the first level shall allow for future conversion to these uses and shall be translucent or transparent. See Façade Standards on Page 18.
- Parking structures shall incorporate architectural articulation to break up long exposed façades.
- When parking structures are located at street corners, corner architectural elements shall be incorporated such as a corner entrance, signage, glazing, and/or visually exposed stairs.
- Parking Structure façades shall be softened with either landscaping such as vines or trailing plants, or the use of architectural features and color. Architectural screens or techniques shall be provided to mitigate any adverse visual effect of the parking structure to the pedestrians and adjacent properties.
- Lighting within parking structures shall be directed inward to prevent light pollution.
- Parking structures and adjacent sidewalks shall be designed so pedestrians are clearly visible to entering and exiting automobiles.
- Commercial or public parking structures shall be located at the interior of a block or underground and shall not be visible from public rights-of-way, unless the UDC approves an exception. Exception requirement: Ground floor commercial uses, or space adaptable for future commercial use, shall be integrated into parking structures located along public streets.



On-street parking can help businesses with parking needs, create a protective sidewalk environment for pedestrians and reduce traffic speed.



Parking uses should not be allowed to dominate the street frontage. Pedestrian-oriented uses should be encouraged on the first floor of the street-side edges of parking structures.

Parking Structure Standards continued:

- Parking structures –shall be located underground or at the interior of a block. The UDC may approve a street facing parking structure with the following conditions:
 - Buildings shall not have exposed structured parking at the ground floor level.
 - A parking structure façade that faces a public street shall be designed to incorporate contextual architectural elements that complement adjacent buildings or buildings in the area.
 - Parking structure openings shall not exceed 50 percent of the total ground floor façade.

Parking Structure Guideline

- Residential Parking Structures should not front public pedestrian space.
- Shared parking garages are encouraged.

Surface Parking Standards

- Surface Parking – lots shall be located behind or to the side of buildings. (Parking lot design standards are contained in Section 6.201 of the Zoning Ordinance.)
- Surface Parking Screening – Parking lots that front a street shall be separated from the sidewalk by a decorative and durable screen at least 4 feet in height.
- Surface Parking Lots – surface parking lots shall be located at the interior of a block, i.e. behind the buildings. Surface parking lots shall not be located adjacent to streets. If the UDC allows a non-interior block parking lot, the tree canopy coverage shall be 40%.
- Surface Parking Screening – if the UDC allows a surface parking lot that fronts a public street, the lot shall be separated from the sidewalk by a screen with a minimum height of 4 feet.
- Security for Surface Parking Lots – a fence not exceeding 6 feet in height may be used to secure driveways and parking lot entrances if it is not located in the front yard. Chain link and barbed wire fences are not allowed.
- Parking structures shall be accessed from alleys or rear driveways or shall be detached and located at the rear of the site.
- Surface Parking Cap – the number of off-street spaces shall not exceed 100% of the total prescribed by Section 6.201 of the Zoning Ordinance, unless a parking study demonstrates need for additional spaces.

Surface Parking Guidelines

- Surface parking lots that front streets are discouraged; all site plan options that minimize surface lots along public streets should be explored.
- Screening walls attached to buildings should be designed as architectural extensions of the building, constructed of the same materials and style.
- Drop-off and loading areas should be located under the overhang of a building that maintains the urban street edge within the maximum front setback
- Generally: Off-Street Parking Requirement – none, except for properties located within 250 feet of a one- or two- family zoning district. For those properties, the requirements in Section 6.201 of the Zoning Ordinance, reduced by 25% shall apply. Uses within historically significant buildings are exempt. Historical significance will be decided by the city's Preservation Officer.

Overarching Standards and Guidelines

Height Standards

- The height and overall scale of new construction and additions shall be compatible within 50 feet of adjacent historic buildings.
- Step downs in building height, wall plane offsets and other variations in building massing shall be utilized to provide transition when height of new construction exceeds adjacent historic structures by more than two stories. Greater variation in height may be appropriate with appropriate transitions. Height shall be determined by the number of stories. To be measured from finished floor to finished floor per story and to the top of the plate or top of parapet for the upper most story.

Signage

Intent

- To encourage excellence in signage, both as a communication tool and as an art form.
 - To allow and encourage creative and unique sign designs while preventing cluttered and unattractive streetscapes.
 - To provide basic parameters for creative signs that may be as varied and unique as the businesses they represent. The standards do not dictate design. Photographs of sign examples are used to illustrate design concepts, but should not be viewed as an exclusive list of acceptable signs. There are many acceptable ways to conform to the standards.
- In general, effectively designed signs should respond to the site, landscape, and architectural design context within which they are located. Signs should be compatible in scale, proportion, and design with the building's façade and its surroundings.

The sign standards address the following:

- Appropriate locations
- Number of signs allowed on a property
- Maximum area for individual signs
- Height limits
- Color and materials
- Lighting

The following signs are not allowed:

- Off-premise signs are prohibited
- Rooftop signs (signs placed above a roof of a building)
- Standard box cabinet wall signs
- Pole signs with the exception of parking lot pole signs, directional signs, and on-premise signs for properties fronting NE 28th street and Main Street are prohibited.

Comprehensive Building Signage Plan

Commercial signs on a large project with uniform façade and more than one storefront, should relate to each other in terms of height, proportion, color, and background value. Maintaining uniformity among these characteristics reinforces the building façade composition while still retaining each business's identity. A comprehensive sign plan may be required by the UDC.

General Design Standards

Types of Signs

- Creative cabinet signs may be allowed. The UDC will determine the appropriateness of creative cabinet signs.

Location

- Signs shall be positioned to emphasize or accent building elements such as storefront openings or entrances.

Alignment

- Where possible and appropriate, signs shall be aligned with those on neighboring buildings to promote visual order on the block, avoid visual clutter, and enhance legibility.

Dimensions and Scale

- Signs shall be consistent with the human scale of Stockyards buildings and blocks.

Measurement of Sign Area

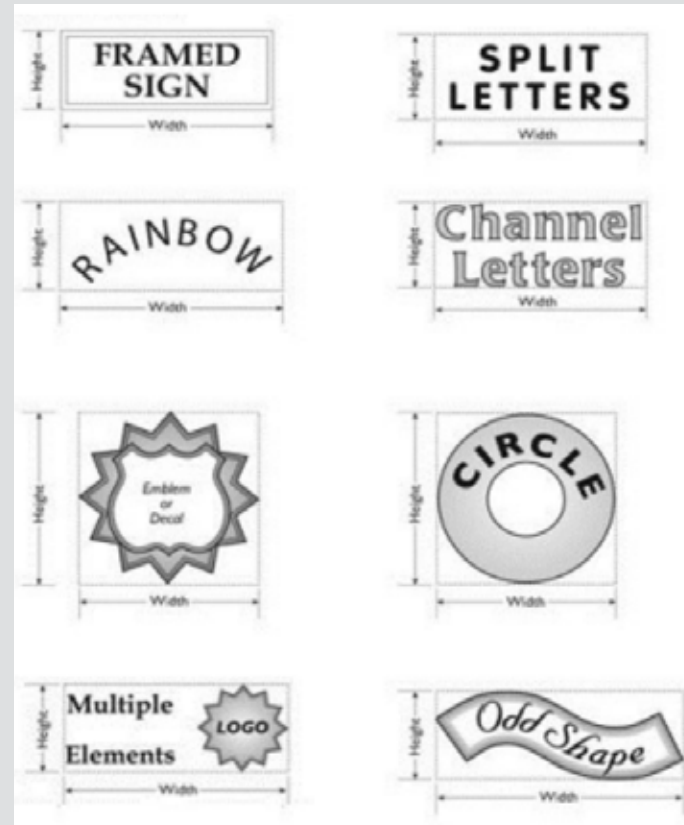
- Sign area for all signs shall be measured by means of the area of one rectangular or circular shape that encloses all sign elements except the support structure.
- For signs consisting of individual letters and/or graphics mounted on a building wall or window, the sign area shall be measured by means of the area of one rectangular or circular shape that encloses all of the letters and graphics that constitute the sign.

Number of Signs

- The number of signs per building façade shall be limited to the fewest necessary to clearly identify businesses located within the building.

Colors and Materials

- Signs shall relate in color and material with the building façade and streetscape. Colors should contribute to legibility and visual appeal.



Measurement of Sign Area

Graphics

- Sign graphics should be simple. Symbols and logos on signs are quickly read and easily remembered. Lettering shall be in proportion to the size of the sign.

Lighting

- Lighting sources shall be external, shielded, and directed only at the sign. Internal illumination is acceptable when the letters themselves, not the background, are lit.

Wall and Awning Sign Standards

Wall signs are attached to and completely supported by exterior walls. A wall sign should be placed above first floor windows on a one-story structure or between windows on multiple story structures. One wall sign for each storefront or building façade that fronts a public street or alley is usually sufficient.

- Maximum width: 66 percent of the linear width of the business.
- Letter height: Lettering shall be in proportion to the size of the sign and the width of the storefront. Wider storefronts could have taller letters. As a general rule, the maximum height of a capital letter should be $\frac{3}{4}$ the height of the sign background.
- Self-illuminated sign awnings are discouraged.

Signs on Side or Rear of Building Standards

- Signs on side or rear building elevations for a ground floor business is permitted provided that the elevation contains a public entrance, fronts on a parking lot, or a street right-of-way.

Second Floor Sign Standards

- Businesses located on a second floor and facing a street can have signs above the second floor windows, with lettering in proportion with the size of the sign and the width of the business.
- Wall signs above the 3rd finish floor line are not allowed. Building identification signs are an exception.

Building Identification Sign Standards

- Limited to one sign per façade
- Limited to two signs per building with one message

Awning and Canopy Sign Standards

Awning and canopy signs are painted on or attached to an awning or canopy above a business door or window. Awning signs may be allowed on the shed portion of the awning, but is preferred on the valence flap. The flap height should be large enough for letters and symbols to be read easily.

- Maximum letter height: 75 percent of the height of the valence flap/shed area.
- Minimum valence height: 8 inches.
- Minimum vertical clearance from sidewalk: 11 feet.
- Material: Matte finish canvas, glass, or metal.
- Color: Solid or striped.

Awning and Canopy Sign Guidelines

- Awning shape should relate to the window or door opening. Barrel shaped awnings should be used to complement arched windows while rectangular awnings should be used on rectangular windows.
- The color of an awning sign should complement the color and material of the building to which it is attached.

Graphic/Logo Signs Standards

Graphic/logo signs bear a brand name, registered trademark or business logo. Signature graphic and logos are allowed to be used as signs for the businesses they represent.

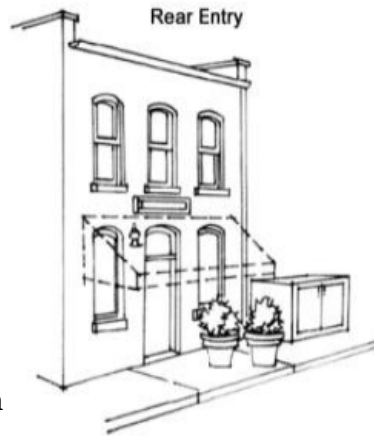
- Graphic/logo signs can be wall signs, awning and canopy signs, or any other sign types identified in these standards.

Projecting Signs Standards

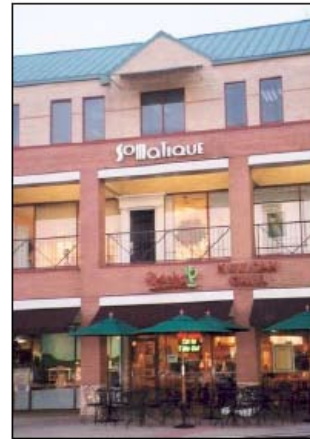
Projecting signs are attached to the face of a building and project more than 12 inches from the wall surface. No more than one projecting sign may be maintained per tenant space frontage at the ground level of a building.

- Minimum vertical clearance: 8 feet.
- Maximum projection: 48 inches.
- Maximum area: 12 square feet.
- Location: Limited to first or second floor. A second or third story tenant with a separate entry door on the street is allowed to have a small projecting sign with a maximum area of 6 square feet near the tenant's street entry.
- Note: The UDC will consider the context and design of larger signs on a case-by-case basis.

Wall and Awning Sign Visual Guidance



Example of a rear building elevation sign.



Examples of upper floor signs.



Awnings and canopies often enhance building architecture, and add color and interest to the streetscape.

Examples of signage that isn't allowed



Supergraphic signs advertising off premise signage



Rooftop sign

Hanging Signs Standards

Hanging Signs are projecting signs suspended below a marquee or canopy. Hanging signs can include logos, symbols, or lettering. A hanging sign is generally intended to be read by pedestrians and by motorists in slow-moving vehicles. To minimize visual clutter, hanging signs shall not be located within close proximity to other hanging signs or projecting signs, preferably at least 25 feet from each other.

- Minimum vertical clearance: 8 feet.
- Maximum projection: 48 inches.
- Maximum area: 6 square feet.
- Location: First floor businesses and upper floor businesses with covered entries, porches, or balconies.

Marquee Signs Standards

Marquee signs are projecting signs attached to the perimeter or border of a permanently roofed building, and are constructed as a part of the building. Marquee signs are usually installed on theatres, cinemas, and performing art facilities.

- Sign copy: Limited to include the facility name and changeable copy related to current and future attractions.
- Maximum area for facility name: 40 percent of total sign area.
- Maximum area for the changeable copy: 80 percent of total sign area.

Window Sign Standards

- Window signs are painted on or attached to the inside of a window facing the street. This type of signage can contain text, graphic logos, and other images.
- Window signs shall not completely obscure visibility into or out of the window. Exceptions may be made for signs that screen utility and office equipment.
- A maximum of one window sign is permitted per window pane or framed window area.
- Temporary window signs are allowed to identify special events and sales provided they are removed immediately following the event.
- Handwritten, paper, cardboard, and plastic signs are not allowed.
- Neon signs are permitted provided that all electrical supply cords, conduit, and electrical transformers are hidden from view through the window.
- Maximum area: 25 percent of the total transparent glass area of windows parallel to the street excluding the area of all glass doors.
- Location: Limited to ground floor window facing the primary street frontage and adjoining parking lot or commercial uses that do not have ground floor occupancy.
- Material: High quality, such as paint, gold-leaf, or neon.



Examples of Hanging Signs



Example of Window Sign

Monument Sign Standards

Monument signs are freestanding and are located adjacent to sidewalks. Such signs are typically used for buildings that are separated from adjacent streets by substantial setbacks. A monument sign shall be set onto a base or frame, presenting a solid, attractive and well-proportioned appearance that complements the building design and materials.

- Maximum area: 60 square feet per sign face with no more than two sign faces.
- Location: Limited to a landscaped area.
- Minimum setback from the public right-of-way: 5 feet.
- Material: Opaque backgrounds with a non-reflective material.
- Lighting: External lighting fixtures designed to complement the appearance of the sign or internal lighting that only illuminates text and logos.
- Maximum height: 8 feet including base.
- Text: Limited to the building or development name, logo, and the business address.
- Background: Opaque and non-reflective.

Tenant Directory Sign Standards

Tenant directory signs are used to identify businesses in multi-tenant buildings that do not have direct frontage on a public street. The design of the sign should complement the building's design.

- Orientation: To the pedestrian.
- Location: Mounted flat against a solid wall within or near a main entrance or incorporated into a freestanding sign located on the property on which the tenants are located.
- Lighting: Directories may be externally illuminated.
- Maximum sign height: 6 feet including the sign base.
- Maximum letter height:
 - 8 inches – Building/development name or logo.
 - 4 inches – All others.



Example of Monument Sign



Example of Tenant Signage

Banner Sign Standards

Banner signs contain a logo or design placed on lightweight material that can move with the wind. A banner sign is intended to add liveliness, color, and a sense of movement to a pedestrian-oriented street. The size of commercial banners should be in scale with the building. Flagpoles and brackets should be placed so as not to obscure architectural elements; locations should be considered in context of the entire façade.

- Orientation: Vertical and mounted perpendicular to the building face.
- Projection: No more than 4 feet.
- Distance above ground: 8 feet at lowest point.
- Banners along the same block of a street should generally be set at the same angle from the buildings.

Business Identification Sign Standards

Wall Sign Standards

- Previous wall sign standards apply.

Monument Sign Standards

- Previous monument sign standards apply.

Canopy Signs Standards

- Maximum area: 24 square feet
- Maximum projection: 12 inches



Example of Wall Sign



Existing Banner Signage



Existing Mural Signage

Temporary Sign Standards

Temporary signs include grand opening banners, community interest signs on private property, construction signs, and real estate signs. Temporary signs are allowed for a limited amount of time, as described below.

Construction Sign Standards

- Signs advertising the various construction firms participating in the development project are allowed.
- The sign area shall not exceed 24 square feet. The sign shall not extend beyond the subject property nor interfere with traffic safety or visibility.
- Illumination is not allowed.

Real Estate Sign Standards

- Real estate signs may be placed in windows only. Signs shall not exceed 10% of the window area or 10 square feet, whichever is larger. The window area shall be calculated by multiplying the window width times the height of the window, with a maximum calculated height of 15 feet.
- Where windows exist on the first floor and the first floor is fully leased, a sign may be placed on the façade if it is integrated with the building architecture.
- Should the building not have windows on the street façade, a real estate sign, not to exceed 10 square feet, may be placed on the façade.
- Larger creative signs that reflect the architecture of the building in placement and design may be considered for new developments, complete renovation of a vacant building, or conversion to new use of existing buildings.
- For buildings with a setback of 20 feet or more, a free-standing sign no larger than 16 square feet per face may be considered. Real estate information is encouraged to be integrated in a monument sign for the building.
- Free-standing signs no larger than 16 square feet per sign face may be placed on vacant properties or parking lots. More than one double-sided or L-shaped sign may be considered if the parcel has 3 block faces or more.



Construction fence is necessary to limit public access to hazardous areas during construction phase and to ensure the safety of both pedestrian and vehicular traffic.

Temporary Signs: A-Frame Sign Standards

Portable A-frame signs are signs or advertising devices, which rest on the ground and are not designed to be permanently attached to a building or permanently anchored to the ground. A-frame signs in the Stockyards are designed to attract pedestrians, not passing motorists.

- Each restaurant is allowed to have one A-frame sign.
- A-Frame signs shall not encroach into required off-street parking areas, public roadways or alleys, and may not be arranged so as to create site distance conflicts or other traffic hazards.
- A-Frame signs can be utilized only during regular business hours and should be removed during non-business hours.
- Location: On private property or within the public right-of-way, provided they do not interfere with vehicle access, pedestrian movement or wheelchair access to, through, and around the site. A minimum access width of 7 feet should be maintained along all sidewalks and building entrances accessible to the public.
- Material: Sturdy and not subject to fading or damage from weather. The use of paper or cloth is not permitted unless located within a glass or plastic enclosure.



Existing Creative Signage

Creative Sign Guidelines

Creative signs bring fun and vitality to the Stockyards streets and are encouraged. The UDC will review creative signage.

Parking Lot Pole Signs

The specifications shown below serve as the basic standards for Stockyards parking lot pole signs. Property owners that design parking lot pole signs to conform to these standards are not required to appear before the UDC.

- Any deviation from this basic design must be reviewed by the UDC.
- Pole signs other than parking lot pole signs, directional signs, and on-premise signs for properties fronting NE 28th Street and Main Street are not allowed.

Guidelines:

- Location – signs should be positioned to emphasize or accent building elements such as storefront openings or entrances.
- Alignment – signs should be aligned with those on neighboring buildings to promote visual order on the block, to avoid visual clutter, and to enhance legibility.
- Dimensions and scale – signs should be consistent with the human scale of the District's buildings and blocks.
- Number of signs – the number of signs per building façade should be limited to the fewest necessary to clearly identify businesses located within the building.
- Colors and Materials – signs should relate in color and material with the building façade and streetscape. Colors should contribute to legibility and visual appeal.
- Graphics – sign graphics should be simple. Symbols and logos on signs are quickly read and easily remembered. Lettering should be in proportion to the size of the sign.
- Lighting – indirect illumination is recommended. Lighting sources should be external, shielded, and directed only at the sign. Internal illumination is acceptable when the letters themselves – not the background – are lit.

Open Space

- Public Park and Plaza Standards
- Park Dedication Policy – the City’s park dedication policy applies to new residential units.
- Plazas – Plazas are urban public spaces that are more formal than parks and have a higher degree of hard surfaces and pedestrian traffic. Guideline for impervious surface area is a maximum of 40 percent.
- Parks – Parks are open spaces that have less hard surface and pedestrian traffic than plazas. Parks typically include both active and passive recreation areas. Guideline for impervious surface area is a maximum of 20 percent.
- Hours – Publicly and privately maintained public park spaces shall be publicly accessible during daylight hours. Access may be limited at other times.

Public Park and Plaza Guidelines

- Design all public spaces other than large recreational parks to be human-scale and visually interesting.
- Locate and design public spaces so that they are clearly visible and easily accessible.
- Incorporate space-defining and active edges – such as multi-story façades with ground floor restaurants – but avoid blank walls. Restaurants and shops that attract pedestrians are ideal edge uses.
- Plan for prevailing sun angles and climatic conditions.
- Avoid locating public spaces on corner lots. Corners are better suited for buildings that establish a strong urban street edge.
- Avoid sunken plazas and architectural bench arrangements; these often fail to promote use.
- Provide flexible space for programmed uses, but design such spaces so that they do not appear barren when there is no programming.
- Incorporate features that attract users, such as fountains, public display areas, or interactive sculpture.
- Incorporate landscaping to provide natural shade and/or to soften hardscape areas.



Existing Public Art in the Stockyards



Existing Public Greenspace

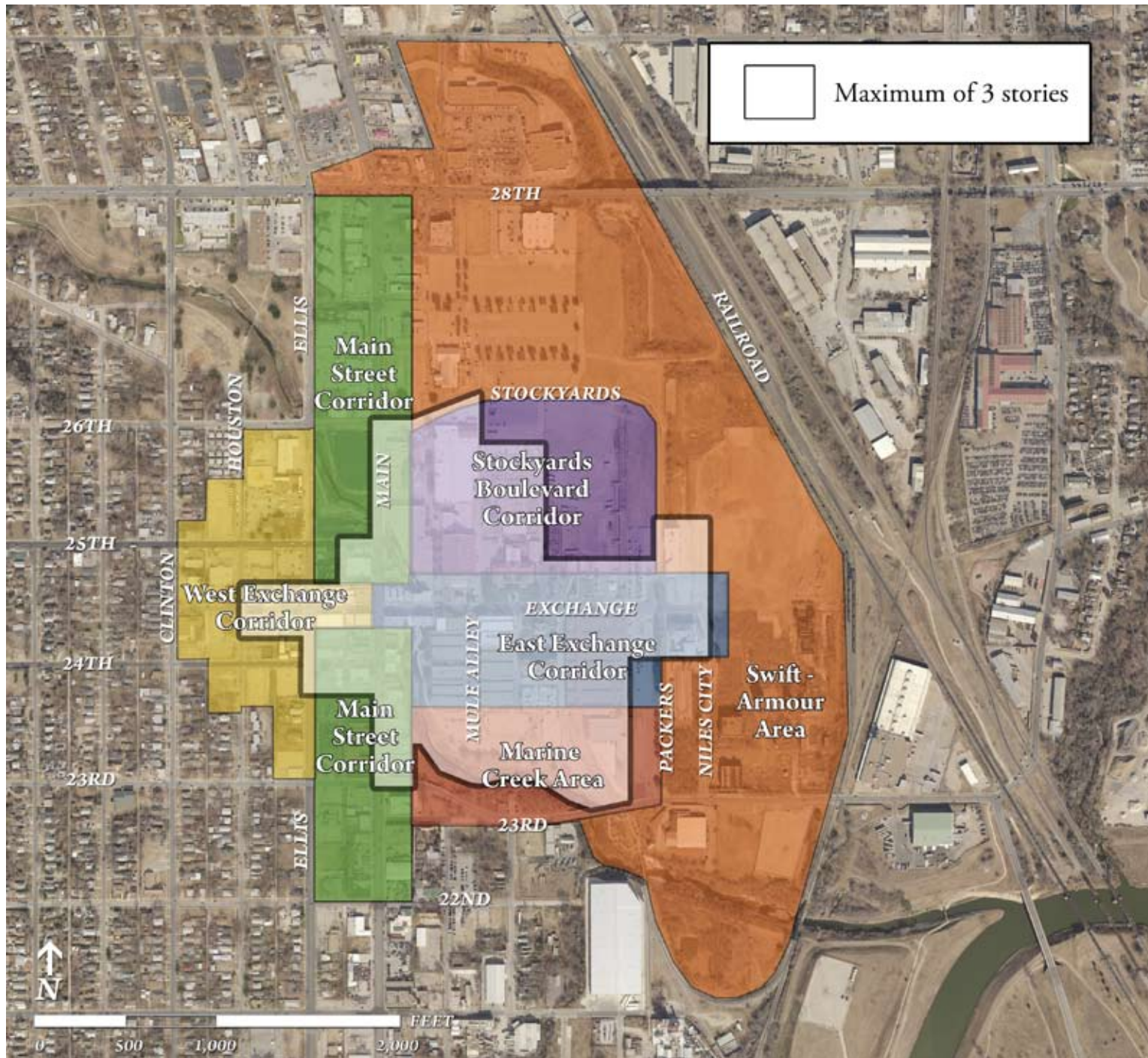


Exhibit C - Corridor/ Area Map with Height Boundary Overlay

Corridor/ Area Specific Guidelines

See Exhibit C for detailed height and district boundaries and how they relate by corridor/area.

Main Street Corridor

Building Guidelines

General Setbacks

- Buildings should have a setback of 0 feet from the back of the sidewalk/property line along Main Street.

Building Height

- Buildings that orient to Main Street should have a minimum building height of 2 stories and a maximum of 3 stories as indicated within the boundary shown on Exhibit C.
- All other heights within the district boundary will be dictated by base zoning.

East Exchange Corridor

Building Setback Standard

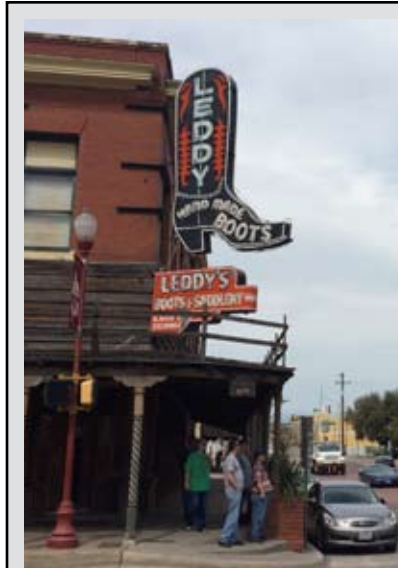
- Buildings on the north side of the street within 50 feet to the Coliseum and Exchange Building shall not have a front yard setback less than the line created by the facades of the 2 historic buildings.

General Setback Guideline

- Buildings on the south side of the street should have a setback of 0 feet from the back of the sidewalk/property line along East Exchange Avenue.

Building Height Guidelines

- Buildings that orient to Exchange Avenue should have a minimum building height of 2 stories and a maximum of 3 stories as indicated within the boundary shown on Exhibit C.
- All other heights within the district boundary will be dictated by base zoning.



Main Street and
East Exchange
Avenue



East Exchange Avenue



View Corridor looking east down Exchange Avenue

West Exchange Corridor

Building Guidelines

General Setbacks

- Buildings should have a setback of 0 feet from the back of the sidewalk/property line along West Exchange Avenue.
- All other front yard setbacks should align with line of prominent block setback.

Building Height

- Buildings shown within the boundary on Exhibit C that orient to Exchange Avenue should have a minimum building height of 2 stories and a maximum of 3 stories.
- All other heights within the district boundary will be dictated by base zoning.

Stockyards Boulevard Corridor

Building Guidelines

Building Height

- Buildings on the south and west side of the Stockyards Boulevard Corridor should have a minimum building height of 2 stories and a maximum of 3 stories as shown within the boundary on Exhibit C.
- All other heights within the district boundary will be dictated by base zoning.

Swift-Armour Area

Building Guidelines

Building Height

- Buildings within the Swift-Armour Area at the end of East Exchange Avenue at the top of the stairs on Packers should have a minimum building height of 2 stories and a maximum of 3 stories as shown within the boundary on Exhibit C.
- All other heights within the district boundary will be dictated by base zoning.



Existing views in the Stockyards Boulevard Corridor

Marine Creek Area

Waterways Standards and Guidelines

Waterfront Access Principles - *Exclusive to the Marine Creek Area*

- Ensure a continuity of public access to and along waterfronts.
- Provide pedestrian and view corridors that allow physical and visual access to the waterfront.
- Clearly define access points and edges of waterfront spaces to facilitate public use and security, and to clearly distinguish between public and semi-public or private spaces.
- Accommodate multiple travel modes along the waterfront, including pedestrians, bicycles (slower speeds along urban waterfronts), and water taxis.

Waterfront Experience Principles - *Exclusive to the Marine Creek Area*

- Enhance a linear park system
- Build a distinctive and diverse network of public spaces and waterfront environments that avoid long uninterrupted monotonous designs.
- Include both informal and formal spaces and their neighborhood connectors to reflect or complement the adjoining neighborhoods character.
- Ensure comfort during hot weather with trees and other provisions for shade, water fountains, benches, and areas for rest.
- Ensure that buildings and other components of the built environment frame and complement waterfronts and public spaces.



Marine Creek Corridor/ Saunderson Park

Marine Creek Area

Building Guidelines

General Setbacks

- There should be a minimum of a 10' setback from Marine Creek improvements along water's edge.

Building Height

- Properties that orient on the north side (that back up to the Mule Barns) and south side of Marine Creek and/or NE 23rd Street should have a minimum building height of 2 stories and a maximum of 3 stories as shown in the boundary on Exhibit C
- All other heights within the district boundary will be dictated by base zoning.

Development Standards for Marine Creek Area

- In order to create a strong “street wall” (the alignment of facades along a Pedestrian Way), buildings shall extend to the sidewalk/ property line and shall have “permeable” walls (walls with windows and primary building entrances) along the sidewalk.

Development Guidelines for Marine Creek Area

- New buildings should complement the historic nature of the surrounding area.
- Where feasible, provide access down to the lower walkway at creek side.

Parking Location Standard

- Waterfront Parking – Surface parking lots and above grade parking structures shall not face the waterfront or greenbelt edge.

Waterfront Walkway Standard

- A continuous walkway or public use area shall be located along the waterfront.

Water's Edge Guideline

- Building ornamentation should be minimized so as to reduce distractions of views and vistas.



Marine Creek Corridor/ Saunder's Park

APPENDIX

A: RECOMMENDED TREE LIST

Large Species

Shumard Oak*	<i>Quercus shumardii</i>
Texas Red Oak*	<i>Quercus texana</i>
High Rise Live Oak*#	<i>Quercus virginiana</i> 'Highrise'
Allee Elm#	<i>Ulmus parvifolia</i> 'Emer II'
Eastern Red Cedar*#	<i>Juniperus virginiana</i>
Western Soapberry*	<i>Sapindus saponaria</i>
Monterrey Oak#	<i>Quercus polymorpha</i>
Bald Cypress*#	<i>Taxodium distichum</i>
Pond Cypress	<i>Taxodium ascendens</i>
Cedar Elm*#	<i>Ulmus crassifolia</i>
Texas Ash*	<i>Fraxinus texensis</i>

Medium Species

Ginkgo (male)#	<i>Ginkgo biloba</i>
Lacebark Elm#	<i>Ulmus parvifolia</i>
Goldenrain Tree	<i>Koelreuteria paniculata</i>
Chinese Pistache#	<i>Pistacia chinensis</i>
Red Maple*#	<i>Acer rubrum</i>
Oklahoma Redbud#	<i>Cercis reniformis</i> 'Oklahoma'

Small Species

Crepe Myrtle#	<i>Lagerstroemia indica</i>
Desert Willow*#	<i>Chilopsis linearis</i>
Possumhaw	<i>Ilex decidua</i>

Trees that should not be considered include:

Bur Oak	<i>Quercus macrocarpa</i>
Sweet Gum	<i>Liquidambar styraciflua</i>
Sycamore	<i>Platanus occidentalis</i>
Bradford Pear	<i>Pyrus calleryana</i>
Southern Magnolia	<i>Magnolia grandiflora</i>

* Texas natives

Drought tolerant species

B: DEFINITIONS

HISTORIC RESOURCE:

Properties that are eligible for listing in the National Register of Historic Places possess historic significance and integrity as determined by the City's Historic Preservation Officer. Additionally, those districts, buildings, structures/infrastructure, objects, sites determined to be historic by the same process. Significance can come from association with historic events or activities, association with important persons, distinctive design or physical characteristics, or the potential to reveal information about prehistory or history. Its integrity must be evident through historic qualities including location, design, setting, materials, workmanship, feeling, and association. Generally properties must be fifty years of age or more to be considered historic places.

DEMOLITION DELAY:

An act or process which destroys a site or structure in its entirety, or which destroys a part of a site or structure and permanently impairs its structural, historic or architectural integrity.

Chapter 9 of the City of Fort Worth Zoning Ordinance

Used to prevent the demolition of buildings or structures that have already been designated as historic landmarks or as contributing structures in a historic district for a specific amount of time, usually ranging from 6 to 24 months. During that time, the preservation commission, preservation organizations, concerned citizens, and others may explore alternatives to demolition, such as finding a purchaser for the structure or raising money for its rehabilitation. These provisions are typically used by communities that lack the authority to deny demolition permits. For example, in North Carolina, local jurisdictions generally only have the authority to delay a demolition permit up to 365 days unless the structure at issue has been determined by the State Historic Preservation Officer to have "statewide significance." See N.C. Gen. Stat. § 160A.400.14.

Reference: National Trust for Historic Preservation

HISTORIC OR CULTURAL LANDMARK:

A structure or property which is of value in preserving the historical, cultural, architectural or archaeological heritage, or an outstanding example of design or a site closely related to an important personage, act or event in history. Such structures or property should be preserved and restored to their historical character and should be protected from modifications which detract from their historical significance.

Chapter 9 of the City of Fort Worth Zoning Ordinance

C: REGULATIONS THAT APPLY TO INDIVIDUAL DEVELOPMENT PROJECTS:

Zoning Ordinance

Minimum Building Standards Code

Urban Forestry Ordinance

Preservation Ordinance

Subdivision Ordinance

D: DESIGNATIONS FOR RESOURCES WITHIN THE BOUNDARIES OF THE NATIONAL REGISTER - LISTED FORT WORTH STOCKYARDS HISTORIC DISTRICT

(Although within the boundaries of the district, this list excludes recent infill north of the North Main Street Marine Creek Bridge)

Address	Historic Name	National Register Contributing	National Register Noncontributing	Recorded Texas Historic Landmark	State Antiquities Landmark	Local Designation	Documented in Tarrant County Historic Resources Survey
111 NW 24 th St	Stockyards Station Branch Post Office	x					x
2400 Ellis Ave	Llano Hotel	x				DD	x
2415 Ellis Ave	Exchange Hotel	x				HC	x
2458 Ellis Ave	Maudie's Cafe		Not extant			DD	x
100 Block E. Exchange Ave	Fort Worth Stock Yards Sign	x		x	x		x
100 Block E. Exchange Ave	Marine Creek Bridge	x					x
100-02 E. Exchange Ave	Maverick Hotel	x				DD	x
101-09 E. Exchange Ave	Thannish Block	x		x		DD	x
104-06 E. Exchange Ave	Woodhouse Building	x				DD	x
110 E. Exchange Ave	Commercial Building		x				
112 E. Exchange Ave	Commercial Building		x				
115-19 E. Exchange Ave	Fort Worth Stock Yards Co. Commercial Building/Stock Yards National Bank	x				DD	x
118-20 (120) E. Exchange Ave	Fort Worth Stock Yards Co. Commercial Building/ Saddle and Sirloin Club	x				DD	x
122 E. Exchange Ave	Horse and Mule Barns (East)	x				HSE	x
122 E. Exchange Ave	Horse and Mule Barns (West)	x				DD	x
123 (121) E. Exchange Ave	Coliseum	x			x	HC	x
123 (121) E. Exchange Ave	Coliseum Ticket Office	x					x
130 E. Exchange Ave	Hog & Sheep Pens	x				HC	x
131 E. Exchange Ave	Fort Worth Livestock Exchange	x		x		HSE	x
200 Block E. Exchange Ave, north side of street	Livestock Pens	x					x
500-600 E. Exchange Ave	Exchange Avenue Square and Armour & Swift Plaza	x			x		x
600 E. Exchange Ave	Swift Offices	x				HSE	x
601 E. Exchange Ave	Armour Building	x				DD	x
101-17 W. Exchange Ave	O'Keefe-Long Commercial Building	x				DD	x
108-12 (110) W. Exchange Ave	Bober Building	x				DD	x
109-11 W. Exchange Ave	Commercial Building	x				DD	x
114-18 W. Exchange Ave	Commercial Building	x					x
115-25 W. Exchange Ave	Commercial Building	x				DD	x
120 W. Exchange Ave	Gray's Bar & Cafe	x				DD	x
124 W. Exchange Ave	Edelbrook Commercial Building/Stockyards Lodge No. 1244	x				DD	x
200 W. Exchange Ave	James F. Dill Grocery	x				DD	x
204-08 W. Exchange Ave	Commercial Building	x					
210-14 W. Exchange Ave	Commercial Building	x					
221-25 W. Exchange Ave	Cody House/ Hereford Hotel	x				DD	x
222 W. Exchange Ave.	Ashby & Dill Grocery/Alps Hotel	x				DD	x

D CONTINUED:

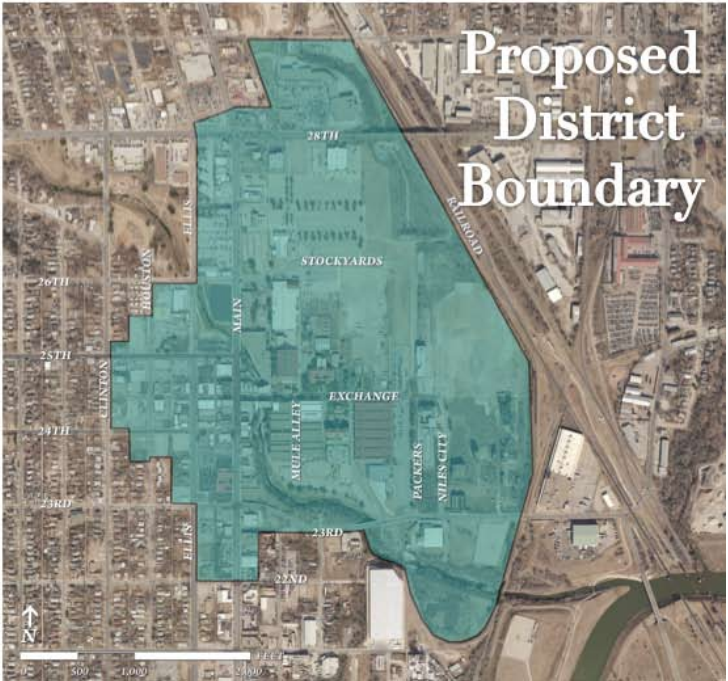
300 W. Exchange Ave	Stockyards Station Branch Post Office/ Boarding House	Outside boundaries of NR district. If boundaries expanded, it would be contributing				DD	x
2322 N. Main St	Commercial Building		x				
2326 N. Main St	Commercial Building	x					
2340 N. Main St	Commercial Building	x					
2350 N. Main St	Commercial Building/A.F.L.-C.I.O. Union Hall	x				DD	x
2356 N. Main St	Motor Supply & Brake Co.	x				DD	x
2358 N. Main St	Alderson Millinery Shop	x				DD	x
2400 N. Main St.	Mitchell's Store/Jacobson's Bargain House	x				DD	x
2401-03 N. Main St.	New Isis Theater	x				DD	x
2402 N. Main St	Stock Yards Tire Shop	x				DD	x
2404-06 N. Main St	Hardwicke Commercial Building	x				DD	x
2405-09 N. Main St	Commercial Building	x					
2408 N. Main St	Stockyards Lodge No. 1244/New Worth Hotel	x				DD	x
2411-13 N. Main St	Commercial Building	x				DD	x
2453-55 N. Main St	O'Keefe-Long Commercial Building/Leddy Boots and Saddle	x				DD	x
2456 N. Main St	Cattlemen's Steakhouse	x					
2457 N. Main St	Lehman Dry Goods Co.	x					x
2601 N. Main St	D. Hart & Sons Livestock Co./Fort Worth Stock Yards Co. Carpenter Shop/Ryan's Saddle and Ranch Supply	x				DD	x
2629 & 2633 N. Main St	D. Hart & Sons Livestock Co./Fort Worth Stock Yards Co. Warehouse	x				HC	x
2517 Rodeo Plaza	Merchants Exhibit Building	x				DD	x
2520 Rodeo Plaza	Cattle Exhibit Building (Billy Bob's)	x				DD	x
East side of 2600 & 2700 blocks N. Main St	Landscape features	x					x
Along NE 23 rd St	Landscape features	x					x

In addition, the Texas Historical Commission holds covenants on "Scale House A" that are in effect until December 31, 2032.

Local Designations:

DD Demolition Delay
 HC Historic and Cultural Landmark
 HSE Highly Significant Endangered

E. BOUNDARY REFERENCES



F: EXTERNAL BUILDING MATERIALS LIST FOR REFERENCE

Primary Materials:

- Brick
- Stone, or stone veneer with cavity wall construction
- Stucco
- Metal Panels – individual or curtain wall systems
- Concrete – finish should be to an architectural level
- Wood
- Cement composite board (not simulating wood)
- Tile – terra cotta, porcelain, or ceramic

Limited Accent and Trim Materials:

- All primary materials listed above
- Metal – galvanized, painted, or ornamental
- Pre-cast masonry (trim and cornice only)
- GFRC (Glass Fiber Reinforced Concrete)
- Glass curtain wall system
- Concrete fiber simulated wood siding

Inappropriate Materials:

- Applied stone without cavity wall construction
- Vinyl or aluminum siding
- Mirrored or highly reflective glass